

ORDINANCE NO. 4370

AN ORDINANCE OF THE CITY OF MESQUITE, TEXAS, AMENDING THE MESQUITE COMPREHENSIVE PLAN THEREBY MODIFYING THE LAND USE POLICIES AND RELATED MAPS PERTAINING TO THE BIG TOWN GATEWAY AND SKYLINE-FORNEY INDUSTRIAL DISTRICT AS APPROPRIATE FOR TRUCKING AND INTERMODAL SERVICES; REPEALING ALL ORDINANCES IN CONFLICT WITH THE PROVISIONS OF THIS ORDINANCE; PROVIDING A SEVERABILITY CLAUSE; AND DECLARING AN EMERGENCY.

WHEREAS, the junction of IH-30 and U. S. Highway 80 is a major gateway to Mesquite from Dallas and other places west; and

WHEREAS, it is also the last impression of Mesquite for westbound traffic before it enters an uninviting, transition corridor in east Dallas; and

WHEREAS, the Big Town Gateway (BTG) area blends seamlessly into the Skyline-Forney Industrial District (S-FID) south of Samuell Boulevard; and

WHEREAS, together the BTG and S-FID form a concentrated logistics hub, which with a new regional identity, could bring new economic development opportunities for west Mesquite; and

WHEREAS, trucking and intermodal services are currently under-represented in Mesquite and the City has little appropriate land to encourage establishment of these uses; and

WHEREAS, the Big Town Gateway and Skyline-Forney Industrial District are ideal locations to encourage the base industry for trucking and intermodal services; and

WHEREAS, the intensity of clustered commercial and industrial uses, with frequent trips from long-haul carriers, conflict with other less intensive uses unless they interact with and provide support for the commercial and industrial uses; and

WHEREAS, industrial corridors create land use, environmental and aesthetic problems if not addressed by proper planning policies and use regulations; and

WHEREAS, the logistics hub must be developed with the highest standards of appearance and give special attention to greening-up an entrance into Mesquite; and

WHEREAS, the Mesquite Comprehensive Plan does not adequately address those land use policies and related maps necessary to improve this area; and

WHEREAS, on July 13, 2015, the Planning and Zoning Commission conducted a public hearing on the proposed amendment pertaining to modifying the land use policies and related maps pertaining to the BTG and S-FID as appropriate for trucking and intermodal services; and

WHEREAS, the Texas Local Government Code, Section 211.004, provides that local zoning regulations must be adopted in accordance with a comprehensive plan, and it is deemed prudent to amend the Mesquite Comprehensive Plan to strongly support and validate the planning policies, principles and approaches that may be employed in the future to help revitalize and redevelop the City's older neighborhoods and commercial corridors.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF MESQUITE, TEXAS:

SECTION 1. That the Mesquite Comprehensive Plan, last updated on March 16, 2015, be and the same is hereby amended by adopting revisions, attached hereto as Exhibit "A," pertaining to the Big Town Gateway and Skyline-Forney Industrial District as appropriate for trucking and intermodal services.

SECTION 2. That all ordinances, or portions thereof, of the City of Mesquite in conflict with the provisions of this ordinance, to the extent of such conflict are hereby repealed. To the extent that such ordinances or portions thereof not in conflict herewith, the same shall remain in full force and effect.

SECTION 3. That should any word, sentence, clause, paragraph or provision of this ordinance be held to be invalid or unconstitutional, the remaining provisions of this ordinance shall remain in full force and effect.

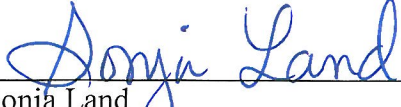
SECTION 4. That the present ordinances of the City of Mesquite are inadequate to provide for the proper protection of the public interest, comfort and general welfare of the City of Mesquite, creates an urgency and emergency for the preservation of the public health, safety and welfare, and requires that this ordinance shall take effect immediately from and after its passage and publication of said ordinance, as the law in such cases provides.

DULY PASSED AND APPROVED by the City Council of the City of Mesquite, Texas, on the 20th day of July, 2015.




Stan Pickett
Mayor

ATTEST:



Sonja Land
City Secretary

APPROVED:



B. J. Smith
City Attorney

Development Areas GATEWAYS

Major entryways or gateways to a community provide first and often lasting impressions of a community's character, quality of life, and economic viability to name a few. These impressions can impact the decisions that individuals, families and businesses make regarding relocating to a community; stopping in or returning to a community to visit, shop, dine, or tour attractions; and what they will tell other people about that community. It is important for Mesquite to guide development and the revitalization of its significant gateways to project a progressive image and the spirit of renewal that drives the community.

Development in gateways shall include strong appearance standards. Careful consideration of permitted land uses is also critical. Each gateway will have its own unique set of characteristics that will influence the types of activities that are appropriate.

Gateway districts are appropriate for any of the major entrances into the city. Two gateways are identified as needing special consideration: The area around the former Big Town Mall site, and Interstate 30 in northeast Mesquite. Other gateways should be evaluated and added to this section as deemed appropriate. In particular, as part of the on-going study for Mesquite's Extra-territorial Jurisdiction, one or more gateways should be identified along the I-20 corridor.

BIG TOWN GATEWAY

The junction of I-30 and U.S. Highway 80 is a major gateway to Mesquite from Dallas and other places west. It is also the last impression of Mesquite for westbound traffic before it enters an uninviting, transition corridor in east Dallas.

An iconic sign is all that remains of the Big Town Mall. The former site of the first enclosed shopping mall in the region was classified for industrial uses in 2014, all but assuring the future character of this gateway for large plate manufacturing and trucking. The area blends seamlessly into the Skyline/Forney Industrial District south of Samuell Boulevard. Together, the Big Town Gateway and the S/FID form a concentrated logistics hub, which with a new regional identity, could bring new economic development opportunities for west Mesquite. Due to the concentration of industrial and commercial uses in this general area, residential use is not considered appropriate. Regardless of the type of use, it must be developed with the highest standards of appearance and give special attention to greening-up an otherwise drab entrance into Mesquite.



I-30 MIXED USE GATEWAY

The eastern entrance to Mesquite along Interstate 30 has several greenfield parcels and other parcels that could be redeveloped. A well-designed gateway at this location with T-5 transect densities and a limited T-6 allocation that produces striking vertical elements can serve to distinguish Mesquite from the low-rise strip development immediately east in Garland. Importantly, a high-quality hotel, retail and employment activity center could provide Dallas County residents with a convenient alternative to the lakeshore developments much farther east. Site design must include an internal street network to maximize value and guard against stripping the Interstate frontage. Opportunities should be explored for linkage to an adjacent

DART park-and-ride facility. Activity centers of this sort are also suitable for high-density residential in a mixed-use setting with ground level retail and office uses. Development in this gateway shall employ new urbanism principles for pedestrian-oriented development and urban civic spaces to provide high aesthetic standards and create the type of dynamic development that progressive cities are moving toward.

